

PUBLIC PLANNING AUTHORITY**Etablissement public d'aménagement Plaine de France**

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MASTER PLANNER**Güller Güller – DHV – EBP – PaulvanBeek**

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Güller Güller architecture urbanism – Masterplan architect

DHV BV – Economic consultant

Ernst Basler + Partner AG – Infrastructural consultant

Paul van Beek landschappen BNT – Landscape architect

Consultants France :

Katalyse – Economic consultant

Setec – Infrastructural consultant

Composante urbaine – Consultant water/agriculture

Safège - Sustainability consultant

Data

| | |
|-----------------------|--------------------------------|
| Site area | 1000 ha |
| Built area | 260 ha |
| 'Cœur du Triangle' | ± 1'700'000 m ² GFA |
| 'Sud-Roissy' | ± 200'000 m ² GFA |
| Lake / urban parks | 60 ha |
| Agriculture / Vallons | 550 ha |

Infrastructural investments (planned / in consideration)

- Métro automatique La Défense - Plaine Saint Denis - Le Bourget - CDG
- Regional railway link RER D-B (Paris – Exhibition Parc Villepinte)
- Lightrail Le Bourget – CDG airport (north-south) / Sarcelles – Villepinte (east-west)
- Urban Boulevard 'BIP' (east-west), with free buslanes
- >50% supply with renewable energy resources as geothermal power, e.a. (objective)
- Innovative waste-water treatment on site

Program 'Cœur du Triangle'

| | | |
|-------------------------------|-----|-------------------------|
| Techno-tertiary business park | 46% | ± 800'000m ² |
| Campus | 15% | ± 250'000m ² |
| Trade boulevard | 8% | ± 150'000m ² |
| Hightech park | 20% | ± 350'000m ² |
| Hotels / convention | 4% | ± 60'000m ² |
| Boarding houses / residences | 3% | ± 50'000m ² |
| Institutes / University | 2% | ± 25'000m ² |
| Services / sport | 2% | ± 30'000m ² |



Paris - Rotterdam, November 2009

Groupement TRIANGLE DE GONESSE

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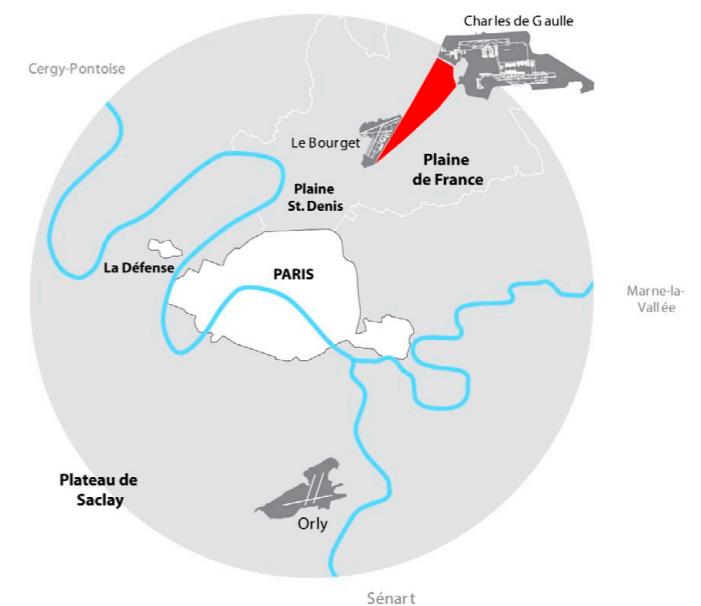


Triangle de Roissy-Gonesse

Airport Corridor Paris-Charles de Gaulle

EPA Plaine de France

Güller Güller – DHV – EBP – PaulvanBeek

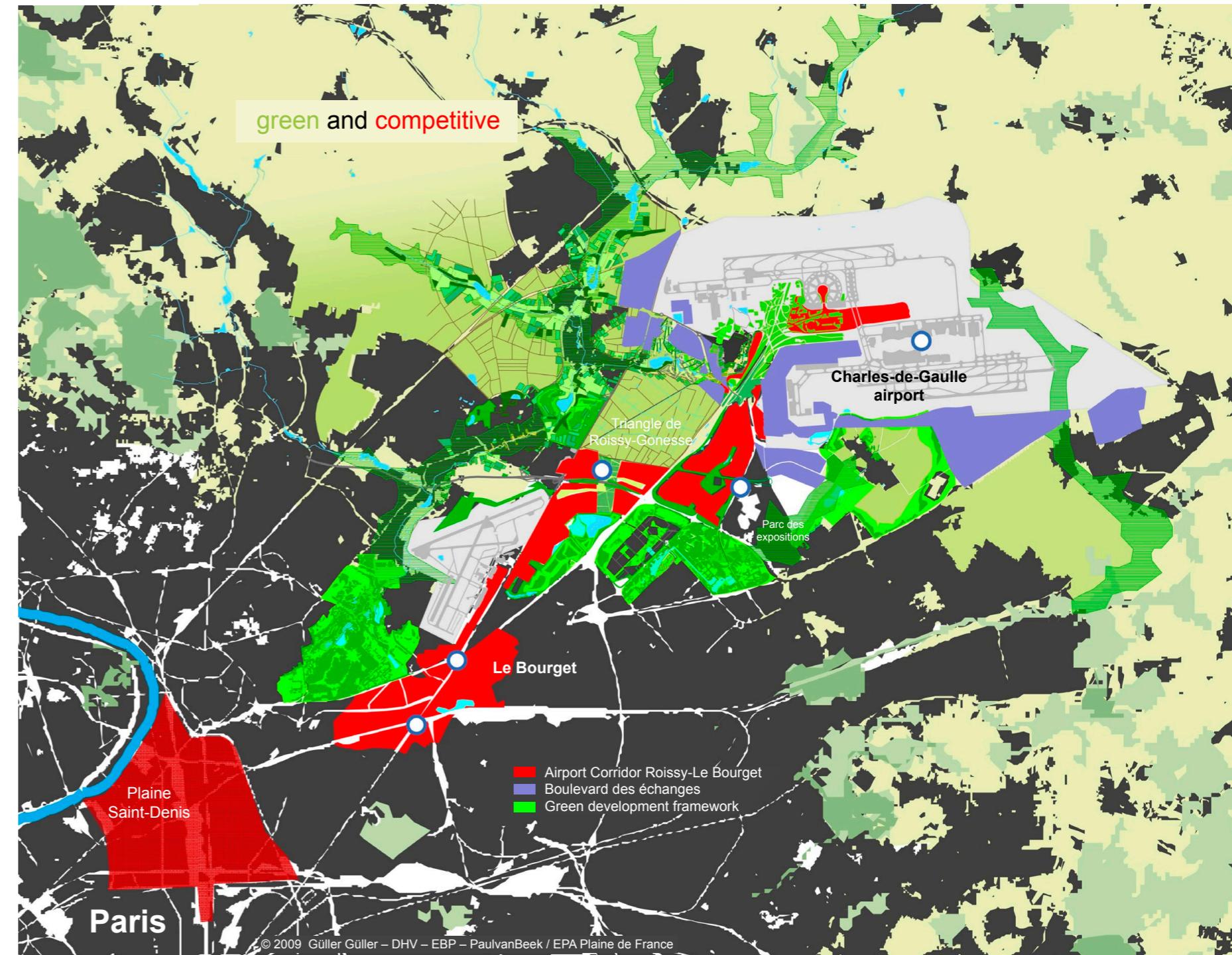


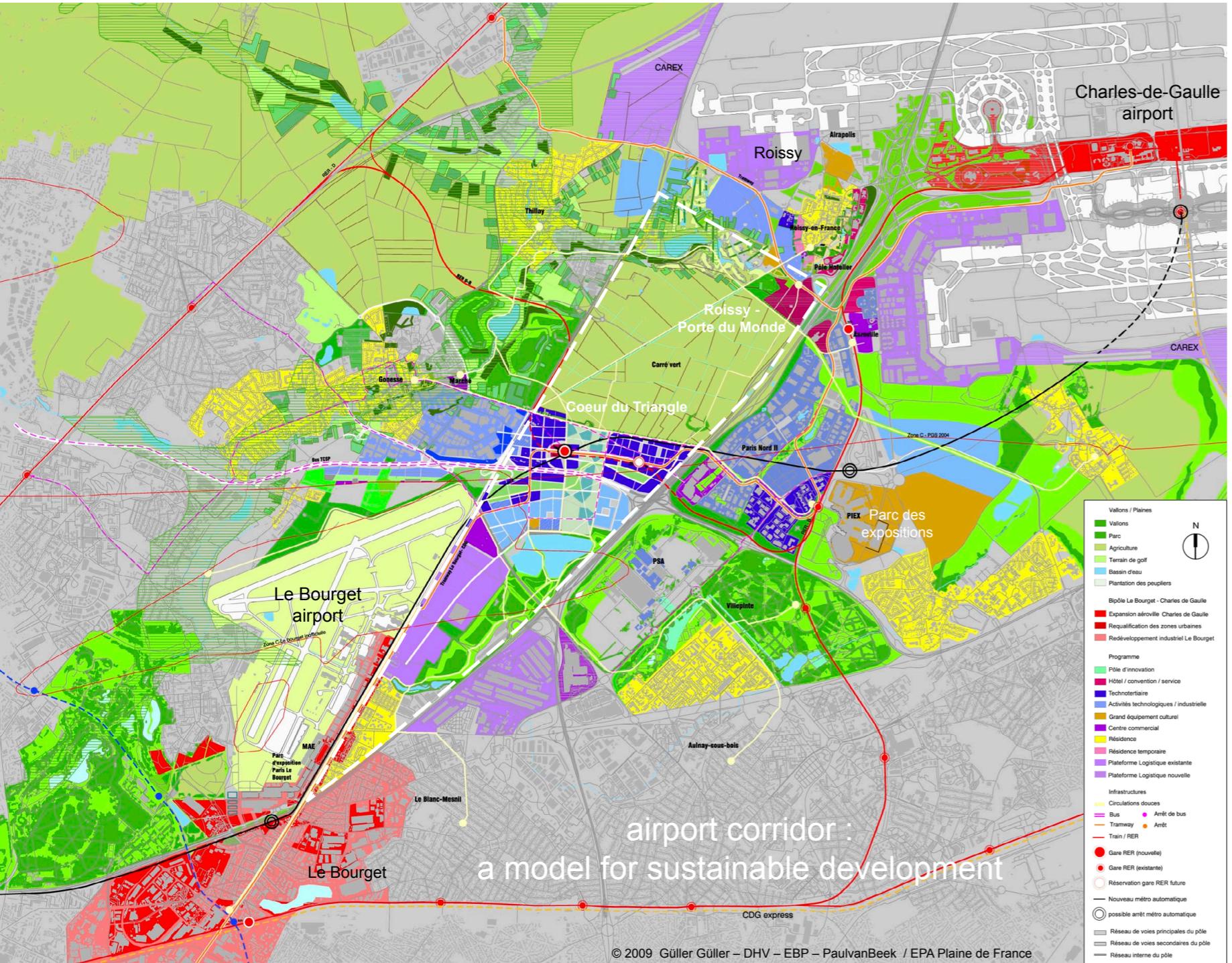
At a time when most European metropolitan areas find their development options around airports increasingly limited, Paris Île-de-France is considering to valorize its main strategic land reserve of the territory of the Plaine de France, the «Triangle de Roissy-Gonesse».

Airports act as engines to the metropolitan and national economy. The Triangle de Roissy-Gonesse will become a unique business environment situated in the heart of the Airport Corridor of Paris - the strategic development axis between the two international airports of Roissy-Charles-de-Gaulle and Le Bourget. Together with the Plateau de Saclay, the Airport Corridor positions itself as one of the key-development sectors of international character of Grand Paris, the Paris region.

The development of the Triangle de Roissy-Gonesse, an area of some 1000 hectares, will play a key role in the twofold ambition to make the airport corridor a center of gravity for the international business community and to accentuate the quality of life in the airport area. This is a unique asset to strengthen the competitiveness of the Île-de-France region, positioning it as a metropolis of excellence.

In 2008, the public planning authority **EPA Plaine de France** has appointed the 'Groupement Güller Güller – DHV – EBP – Paul van Beek', a Swiss/Dutch multidisciplinary team, as master planner and supervisor of the development strategy for the Triangle de Roissy-Gonesse. The master plan will be presented to the public by spring 2010. The first fases of operational planning are to start 2013 and implementation is scheduled to start 2015-2017.





Project description

The Triangle de Roissy-Gonesse will be a key-project to set-up the Paris Airport Corridor and will dot it with the image of a « modern and sustainable city ».

Techno-tertiary

It will potentially accommodate a vast program of approximately 2 million square meters of techno-tertiary activities within the next twenty-five years:

- **Techno**, because technology-driven companies in growth sectors, like clean tech, ICT, life sciences, and creative industry will especially be targeted.
- **Tertiary**, because the zone will be accommodating those business activities of internationally operating companies that are looking for high quality and flexible offices in an inspiring working environment close to the two competitive airports.

Mixed-use

The Triangle de Roissy-Gonesse will have the allure of a dynamic economic landscape. Eventually, a total of 40,000 people will be working in its urban areas 'Coeur du Triangle' and 'Porte du Monde', most of them highly qualified. The Triangle will attract strategic functions of international companies (as R&D centers, European headquarters, training centers, shared service centers), training, trade and exposition activities, and SME's. It will provide with a campus-like environment for knowledge and training institutes, and offer hotels, boarding houses, commercial and cultural equipments, as well as amenities for the neighbouring inhabitants. This contributes to a mixed-use business environment of highly progressive character.

The development of the Triangle de Gonesse will trigger an increasing dynamism, create added value and play the role of a lever to the revalorization of existing business parks in the airport corridor : a 'chain of pearls' dotted with major facilities like the Parc des Expositions, Airapolis and the Musée de l'air et de l'espace.

Compact and sustainable

The Triangle de Roissy-Gonesse is to become a pilot-project of a 'sustainable city'. Its masterplan will distinguish itself in international comparison through its compactness. Such limited land-use allows for preserving a large portion of the existing agricultural landscape. And its compact urban set-up guarantees energy efficiency and optimal accessibility by public transportation: all buildings are within less than 300m walking distance. In short, the key factor for its attractivity lies in its urban set-up aspiring for sustainability.

Public transport accessibility

A new regional railway service RER, the planned metro for 'Grand Paris' and a new light rail line form the backbone of the airport corridor, providing excellent public transportation for the future business-community as well as the inhabitants of the neighboring départements of Val d' Oise, Seine Saint-Denis and the Picardie.

The east-west highway BIP will be transformed into an urban boulevard, becoming the central artery of the plan - a valuable and attractive public space equipped with shops, restaurants and services situated in the heart of the future development.

Second green sector of Paris

Currently, the area around Charles-de-Gaulle airport is not known for its quality of live. Herefore, the plan preserves and requalifies 450ha of the existing landscape to create a 'Carré vert', and aims at stimulating on medium term periurban agricultural activity that serves a regional customer market. This will create added value for economic investment, improve the quality of life and business activity, and qualify the airport area of Charles-de-Gaulle as the 'second green sector' for Paris.

The Triangle de Roissy-Gonesse is poised to become a reference for the necessary change in the development model of our metropolises, triggered by 'post Kyoto' and the current crisis.

